



U.S. Department  
of Transportation  
**Federal Aviation  
Administration**

New England Region  
Airports Division

12 New England Executive Park  
Burlington, Massachusetts 01803

October 17, 2012

James Doar  
Town Manager  
PO Box 1660  
Bethel, Maine 04217

Dear Mr. Doar:

This is to notify you that our office has approved your airport layout plan marked FINAL February 2011 with revision and accepted the *Airport Master Plan Update Report* draft dated May 2009, subject to the following comments and conditions and revisions.

The master plan recommends use of Airport Reference Code B-II to reflect the approach speeds (less than 121 knots) and wingspan (less than 79 feet) characteristics of aircraft that commonly use the airport (500 annual operations is the usual guidance for this determination). The forecast of operations growing from the current 4,520 operations to a 2029 level of 7,500 operations is accepted. There is not any proposed development that would be affected by any error in these operations. Similarly the forecast for based aircraft to grow to 15 is also very modest. While the master plan identifies aircraft parking areas in excess of this forecast, this is prudent to avoid loss of opportunity should demand increase. Changes in based aircraft can change rapidly especially relative to this scale.

I do not approve of any future terminal facility to the terminal area as indicated on the current plan submitted by Oest Associates (AMEC). First, there has been a new hangar built on a location that voids their concept and second, I am in receipt of a revised airport layout plan dated September 2012 by Stantec. The proposed revisions are too extensive for approval without further justification and review and signature approval by the Town,, Maine DOT and FAA.

To resolve this transition I am approving a pen and ink change (see attached drawing) to the May 2009 plan to include the following portions of Stantec's proposed revision:

- Hangar 7 should be shown as "existing".
- Hangars sites 8,9 and 10 are approved subject to airspace review (FAA Form 7460)
- Terminal Building 11 and associated auto parking lot and driveway access to North Road. The existing terminal building should be shown as "to be demolished" with proper restoration of the site.

No new apron pavement or taxiway development is approved until submission and acceptance of a narrative report that justify the depicted facilities and pavement. I request

an alternate layout to depict development of a more limited taxiway /taxiway system to support the next phase of hangar development.

There is a need to clarify the status of Lot 1A which had been released as airport land to the Town and remains in that status on the current Exhibit A. No grant can be issued for the terminal building until we receive an Exhibit A that documents the transfer of this land back to the status of airport property.

The master plan does not provide sufficient evidence to support the runway extension. This requires substantiated evidence that you have 500 or more operations of aircraft that depart with payload penalties or aircraft using other airports for trips originating or ending in your area. We do approve acquisition of any parcels within the current RPZ including the private haul road which obstructs any runway extension. We will accept its depiction as an ultimate airport development and enter its coordinates into our NASR database as a proposed runway end. This will protect its airspace.

Per the Town's request the FAA has conducted the obstruction survey to support developing a public instrument approach. We have airspace approval for the marking of terrain obstructions to Part 77 surfaces. The following projects should receive your priority to support your requested instrument approach and night operations:

- Mark the runway with non-precision markings (required even with daytime only operations).
- Install hazard beacons on terrain as indicated (subject to study).
- Install obstruction lights on on-airport penetrations of Part 77 surface.
- Install an airport rotating beacon (need to identify location).
- Install MIRLS (coincident with required runway reconstruction)
- Install REILS.

The recommendation for PAPIs to provide visual vertical guidance to pilots is encouraged. However these systems must be maintained by the town and will require a certified technician to properly set the lights' alignments.

Advancement of the terminal building in advance of these higher priority projects is approved. These projects require a finding that it is practical to mark the terrain and environmental assessment. This is also in recognition that the town has already transferred expiring entitlement dollars waiting for resolution of the Part 77 analysis.

I urge the next master plan update to evaluate the feasibility of clearing the GQS surface to support instrument approaches with vertical guidance. Although obstructions to the missed approach surface preclude lowering minimums, vertical guidance can significantly enhance safety on a short runway near mountainous terrain.

This approval does not exempt the town or its tenants from obtaining a determination of "No Objection" from the FAA prior to construction of any proposed structure or facility. An airspace study of proposed construction can be entered on line (<https://oeaaa.faa.gov>).

In addition to the preceding comments, approval of this Airport Layout Plan does not imply any commitment on the part of the federal government to provide financial assistance for any of the proposed projects depicted on this plan. This approval is subject to review of the criteria requiring environmental assessment under NEPA prior to initiating any project. Such determination should be submitted either on the New England Region's Project Readiness Form or by separate documentation.

Sincerely,

A handwritten signature in black ink, appearing to read "Ralph Nicosia-Rusin". The signature is written in a cursive, flowing style.

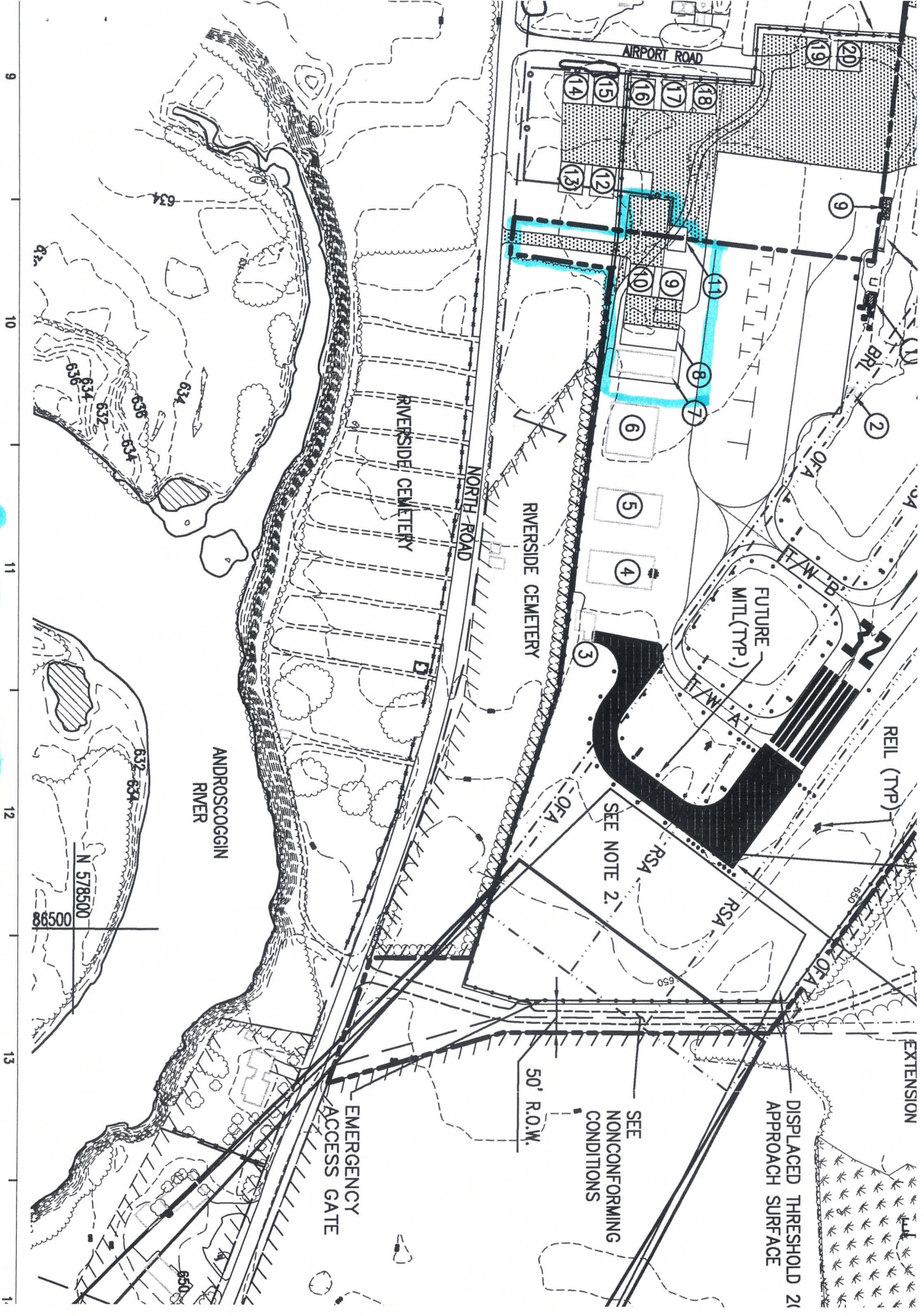
Ralph Nicosia-Rusin  
Airport Capacity Program Manager

Airspace 2011-ANE-104-NRA

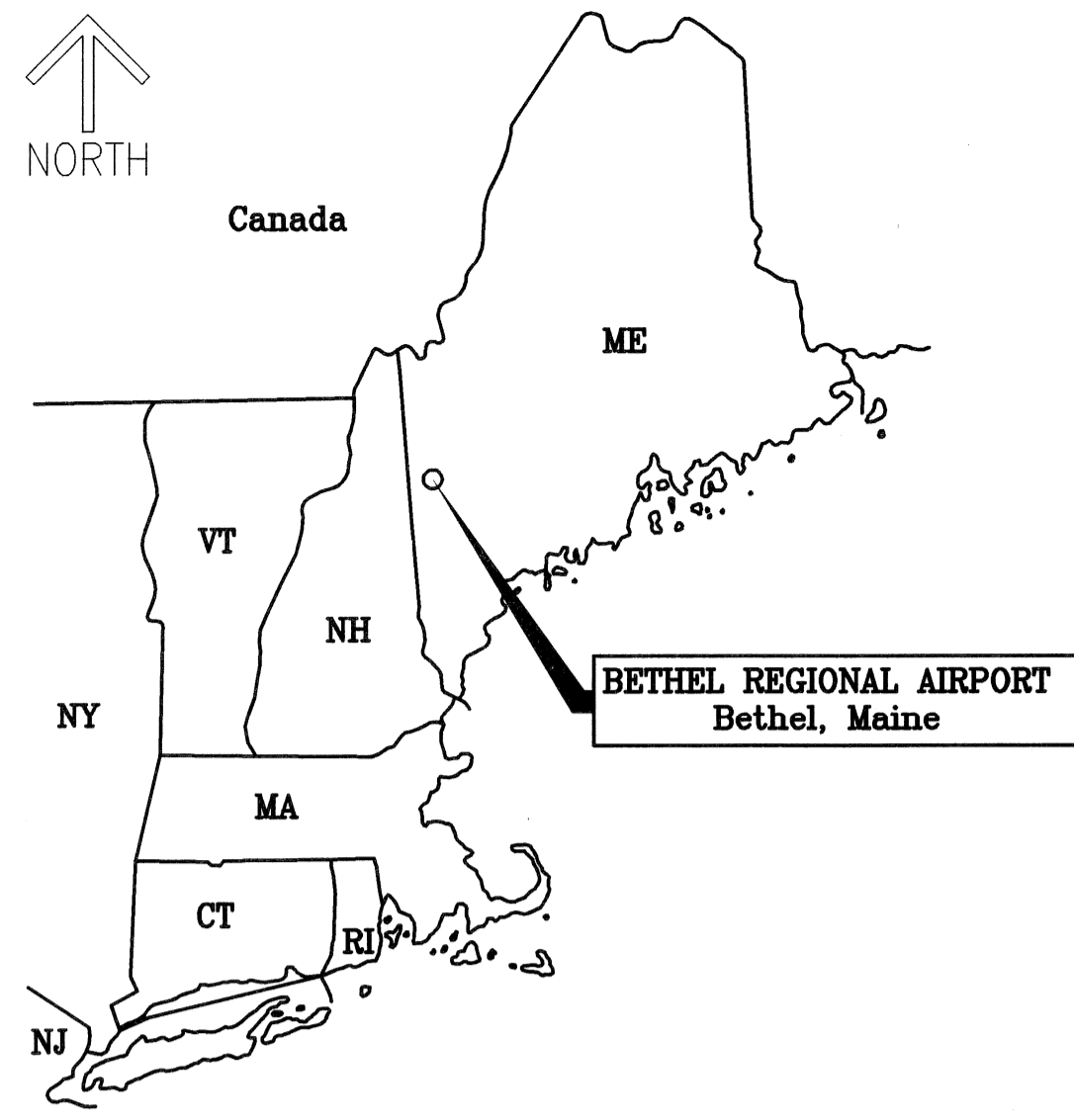
Attached drawing of pen and ink revision approved by this letter,



*Extent of Pen & Ink Change*



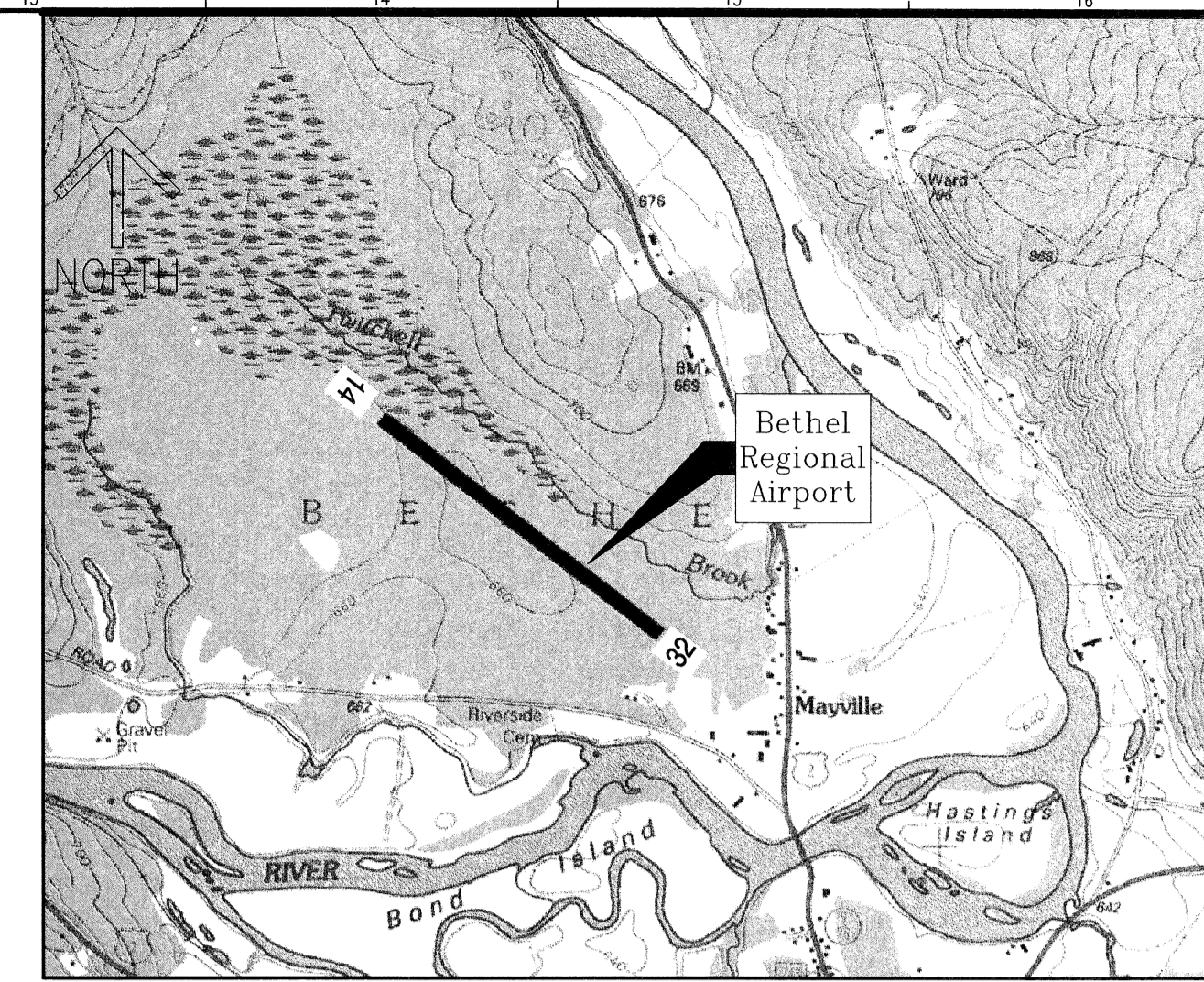




LOCATION MAP  
N.T.S.

# BETHEL REGIONAL AIRPORT

## Bethel, Maine



VICINITY MAP  
SCALE: 1"=2,000'(±)

# BETHEL AIRPORT MASTER PLAN UPDATE

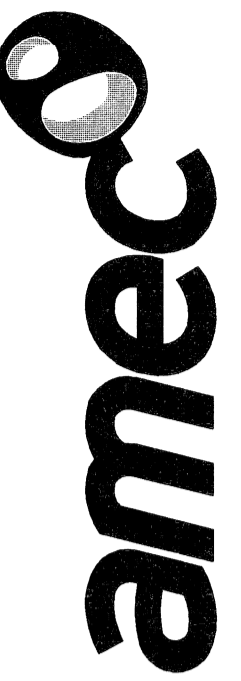
## AIP Project No. 3-23-0008-10-2009

Index of Drawings		
SHEET	TITLE	DRAWING NO.
1	TITLE SHEET	G-101
2	EXISTING AIRPORT FACILITIES PLAN	C-101
3	ULTIMATE AIRPORT LAYOUT PLAN	C-102
4	TERMINAL AREA PLAN	C-103
5	RUNWAY 14 APPROACH PLAN AND PROFILE	C-104
6	RUNWAY 32 APPROACH PLAN AND PROFILE	C-105
7	FAR PART 77 IMAGINARY SURFACES PLAN	C-106
8	LAND USE PLAN	C-107

Prepared by:



Earth & Environmental, Inc.  
343 Gorham Road, South Portland ME 04106  
P: (207) 761-1770 F: (207) 774-1246 www.amec.com



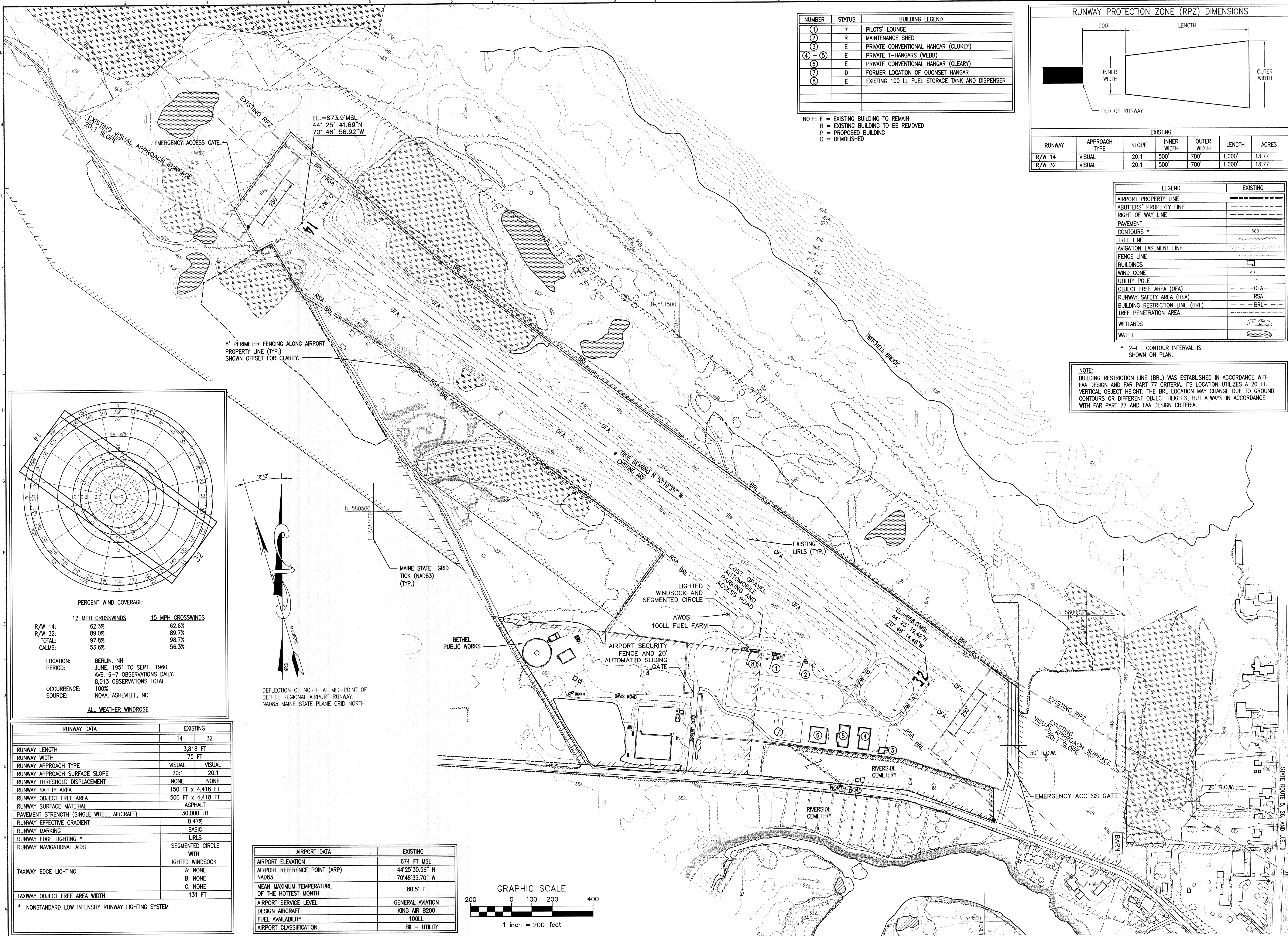
343 Gorham Road, South Portland ME 04106  
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CURRENT ISSUE STATUS:  
**FINAL**  
FEBRUARY 2011  
PROJECT:  
BETHEL REGIONAL AIRPORT  
AIP PROJECT NO. 3-23-0008-10-2009  
BETHEL, MAINE

REV	DATE	REVISION ISSUE DESCRIPTION

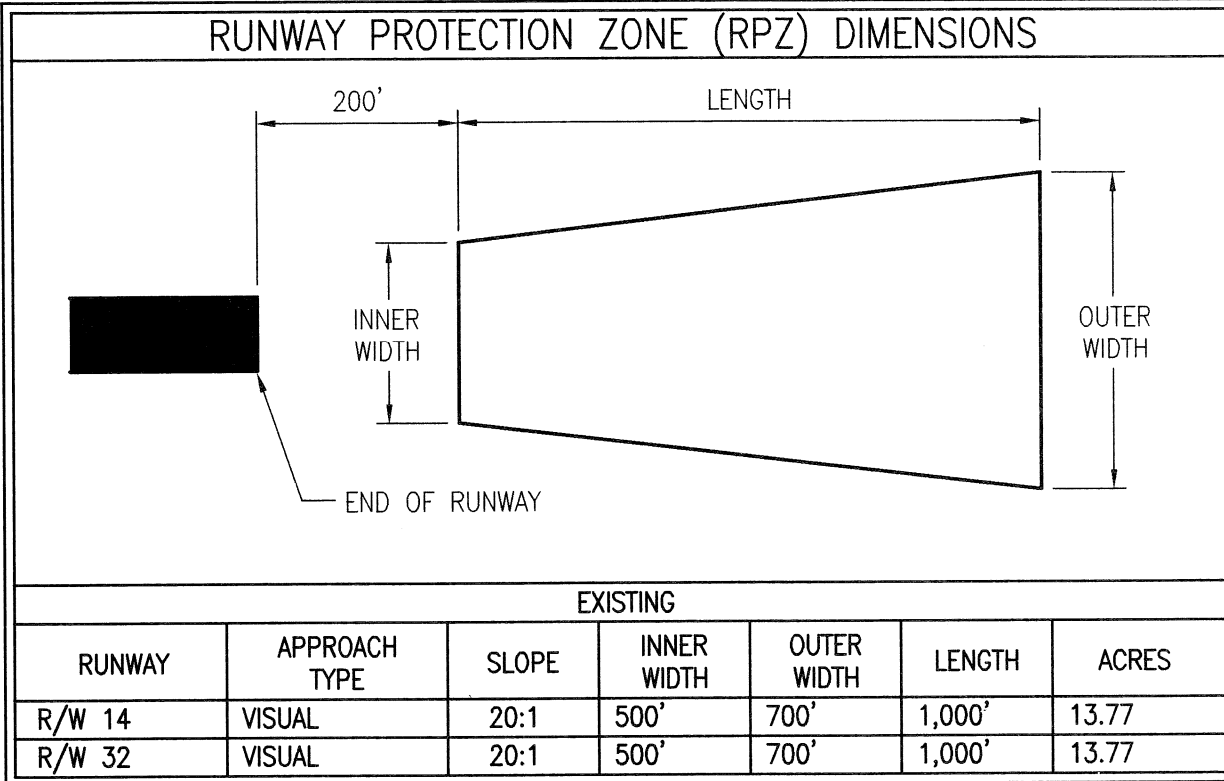
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PROJECT MANAGER: RDY  
A/E OF RECORD: RDY  
DRAWN BY: DAB  
CAD FILE: 2240201-G-101  
SCALE: 1"= 200'  
GRAPHIC SCALE:  
0" 1"  
TITLE:  
TITLE SHEET  
DRAWING NO:  
G-101  
SHEET: 1 OF 8





NUMBER	STATUS	BUILDING LEGEND
1	R	PILOTS' LOUNGE
2	R	MAINTENANCE SHED
3	E	PRIVATE CONVENTIONAL HANGAR (CLUKEY)
4-5	E	PRIVATE T-HANGARS (WEBB)
6	E	PRIVATE CONVENTIONAL HANGAR (CLEARY)
7	D	FORMER LOCATION OF QUONSET HANGAR
8	E	EXISTING 100 LL FUEL STORAGE TANK AND DISPENSER

NOTE: E = EXISTING BUILDING TO REMAIN  
 R = EXISTING BUILDING TO BE REMOVED  
 P = PROPOSED BUILDING  
 D = DEMOLISHED

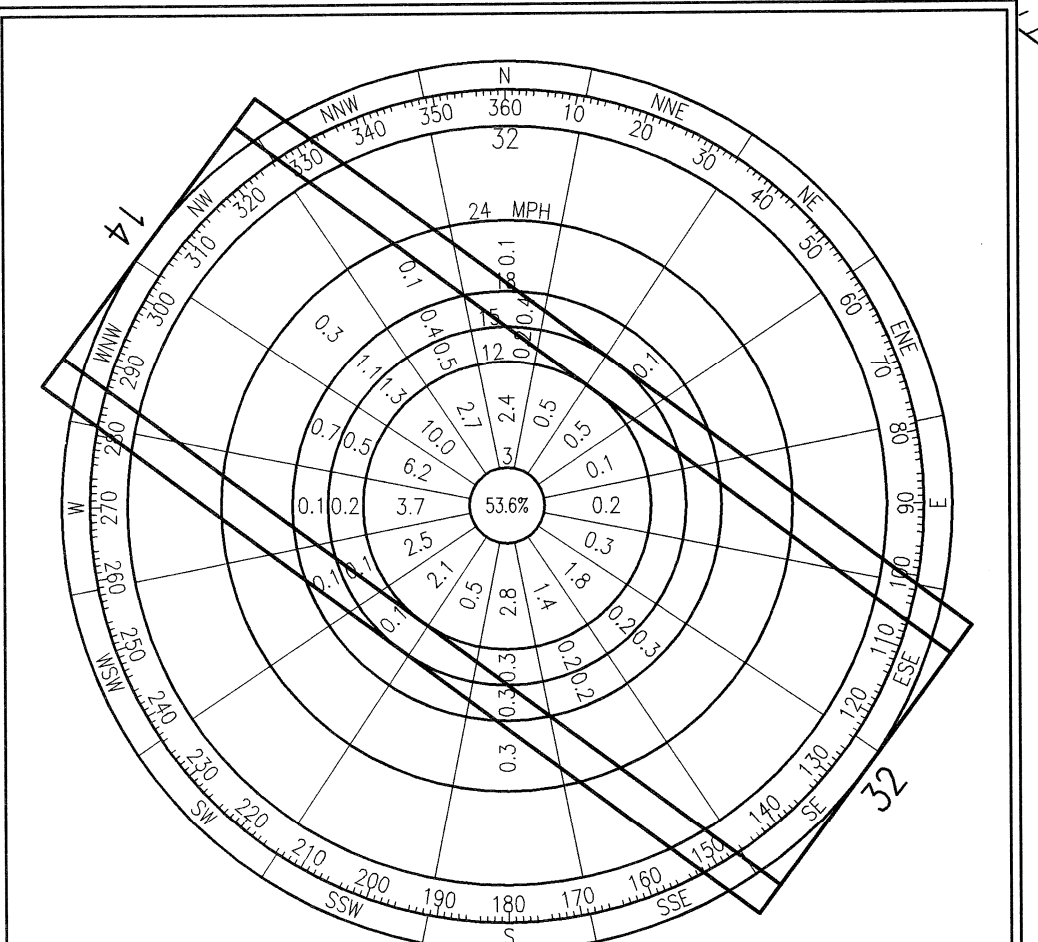


RUNWAY	APPROACH TYPE	SLOPE	INNER WIDTH	OUTER WIDTH	LENGTH	ACRES
R/W 14	VISUAL	20:1	500'	700'	1,000'	13.77
R/W 32	VISUAL	20:1	500'	700'	1,000'	13.77

LEGEND	EXISTING
AIRPORT PROPERTY LINE	---
ADJUTERS' PROPERTY LINE	---
RIGHT OF WAY LINE	---
PAVEMENT	---
CONTOURS *	---
TREE LINE	---
AVIGATION EASEMENT LINE	---
FENCE LINE	---
BUILDINGS	---
WIND CONE	---
UTILITY POLE	---
OBJECT FREE AREA (OFA)	---
RUNWAY SAFETY AREA (RSA)	---
BUILDING RESTRICTION LINE (BRL)	---
TREE PENETRATION AREA	---
WETLANDS	---
WATER	---

\* 2-FT. CONTOUR INTERVAL IS SHOWN ON PLAN.

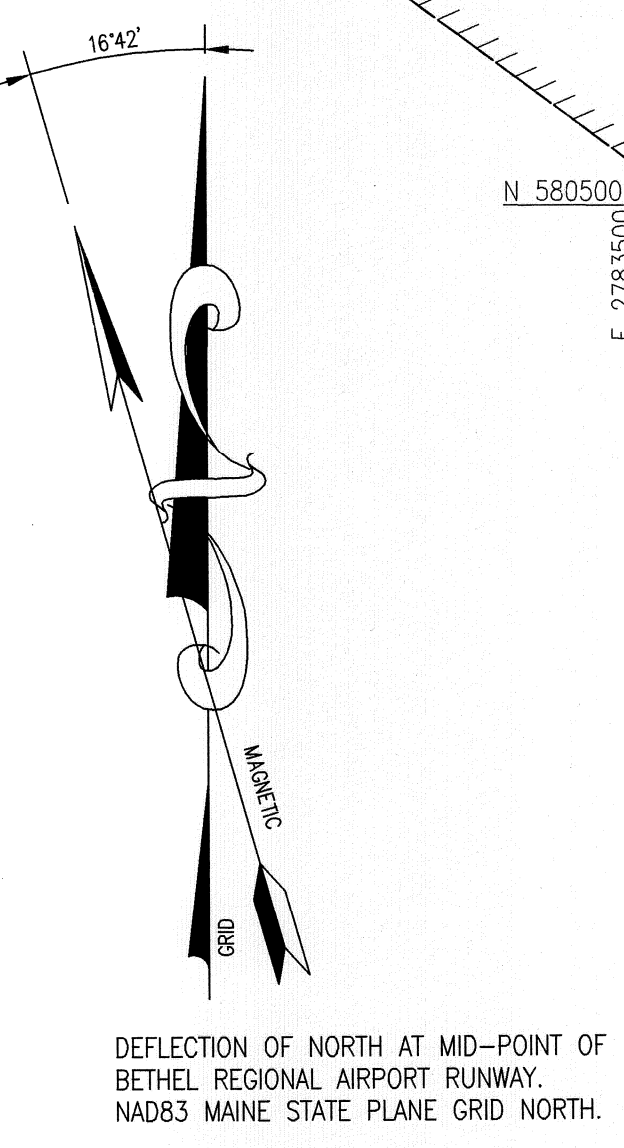
NOTE: BUILDING RESTRICTION LINE (BRL) WAS ESTABLISHED IN ACCORDANCE WITH FAA DESIGN AND FAR PART 77 CRITERIA. ITS LOCATION UTILIZES A 20 FT VERTICAL OBJECT HEIGHT. THE BRL LOCATION MAY CHANGE DUE TO GROUND CONTOURS OR DIFFERENT OBJECT HEIGHTS, BUT ALWAYS IN ACCORDANCE WITH FAR PART 77 AND FAA DESIGN CRITERIA.



	12 MPH CROSSWINDS	15 MPH CROSSWINDS
R/W 14:	62.3%	62.6%
R/W 32:	89.0%	89.7%
TOTAL:	97.8%	98.7%
CALMS:	53.6%	56.3%

LOCATION: BERLIN, NH  
 PERIOD: JUNE, 1951 TO SEPT., 1960.  
 AVE. 6-7 OBSERVATIONS DAILY.  
 8,013 OBSERVATIONS TOTAL.  
 OCCURRENCE: 100%  
 SOURCE: NOAA, ASHEVILLE, NC

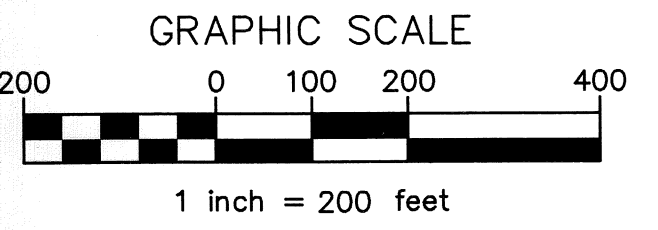
ALL WEATHER WINDROSE



RUNWAY DATA	EXISTING
RUNWAY LENGTH	3,818 FT
RUNWAY WIDTH	75 FT
RUNWAY APPROACH TYPE	VISUAL
RUNWAY APPROACH SURFACE SLOPE	20:1
RUNWAY THRESHOLD DISPLACEMENT	NONE
RUNWAY SAFETY AREA	150 FT x 4,418 FT
RUNWAY OBJECT FREE AREA	500 FT x 4,418 FT
RUNWAY SURFACE MATERIAL	ASPHALT
PAVEMENT STRENGTH (SINGLE WHEEL AIRCRAFT)	30,000 LB
RUNWAY EFFECTIVE GRADIENT	0.47%
RUNWAY MARKING	BASIC
RUNWAY EDGE LIGHTING *	LIRLS
RUNWAY NAVIGATIONAL AIDS	SEGMENTED CIRCLE WITH LIGHTED WINDSOCK
TAXIWAY EDGE LIGHTING	A: NONE B: NONE C: NONE
TAXIWAY OBJECT FREE AREA WIDTH	131 FT

\* NONSTANDARD LOW INTENSITY RUNWAY LIGHTING SYSTEM

AIRPORT DATA	EXISTING
AIRPORT ELEVATION	674 FT MSL
AIRPORT REFERENCE POINT (ARP) NAD83	44°25'30.56" N 70°48'35.70" W
MEAN MAXIMUM TEMPERATURE OF THE HOTTEST MONTH	80.5° F
AIRPORT SERVICE LEVEL	GENERAL AVIATION
DESIGN AIRCRAFT	KING AIR B200
FUEL AVAILABILITY	100LL
AIRPORT CLASSIFICATION	BII - UTILITY



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CURRENT ISSUE STATUS:  
**FINAL**  
**FEBRUARY 2011**  
 PROJECT:  
**BETHEL REGIONAL AIRPORT**  
**AIP PROJECT NO. 9-23-0008-10-2009**  
 BETHEL, MAINE

REV	DATE	REVISION	ISSUE DESCRIPTION

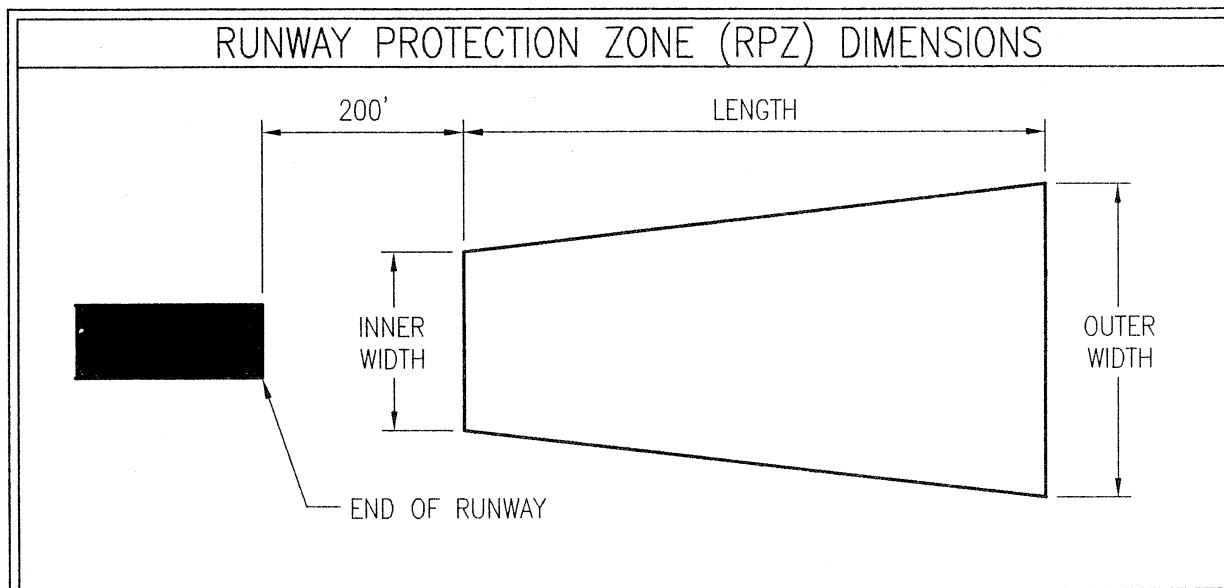
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 PROJECT MANAGER: RDY  
 A/E OF RECORD: RDY  
 DRAWN BY: DAB  
 CAD FILE: 2240201-C-101  
 SCALE: 1" = 200'  
 GRAPHIC SCALE:  
 0" 1"  
 TITLE: **EXISTING AIRPORT FACILITIES PLAN**  
 DRAWING NO: **C-101**  
 SHEET: 2 OF 8



LEGEND	EXISTING	ULTIMATE
AIRPORT PROPERTY LINE	---	---
ABUTTERS' PROPERTY LINE	---	---
RIGHT OF WAY LINE	---	---
PAVEMENT	---	---
CONTOURS	---	---
TREE LINE	---	---
AVIGATION EASEMENT LINE	---	---
FENCE LINE	---	---
BUILDINGS	---	---
WIND CONE	---	---
UTILITY POLE	---	---
OBJECT FREE AREA (OFA)	---	---
RUNWAY SAFETY AREA (RSA)	---	---
BUILDING RESTRICTION LINE (BRL)	---	---
TREE PENETRATION AREA	---	---
WETLANDS	---	---
WATER	---	---

NUMBER	STATUS	BUILDING LEGEND
1	R	PILOTS' LOUNGE
2	R	MAINTENANCE SHED
3	E	PRIVATE CONVENTIONAL HANGAR (CLUKEY)
4-5	E	PRIVATE T-HANGARS (WEBB)
6	E	PRIVATE CONVENTIONAL HANGAR (CLEARY)
7	D	FORMER LOCATION OF QUONSET HANGAR
8	E	EXISTING 100 LL FUEL STORAGE TANK AND DISPENSER
9	P	JET-A FUEL STORAGE TANK AND DISPENSER
10	P	8,000 SQUARE FOOT CORPORATE HANGAR
11-13	P	3,000 SQUARE FOOT HANGAR
14	P	4,800 SQUARE FOOT HANGAR
15	P	TERMINAL BUILDING

NOTE: E = EXISTING BUILDING TO REMAIN  
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RUNWAY	APPROACH TYPE	SLOPE	INNER WIDTH	OUTER WIDTH	LENGTH	ACRES
R/W 14	VISUAL	20:1	500'	700'	1,000'	13.77
R/W 32	NON-PRECISION (APPROACH)	20:1	500'	700'	1,000'	13.77
R/W 32	NON-PRECISION (DEPARTURE)	20:1	500'	700'	1,000'	13.77

DESCRIPTION	STANDARD	EXISTING/PLANNED	REMARKS	DATE APPROVED
RUNWAY 32 SAFETY AREA	150' X 300'	PLANNED	NORTHEAST CORNER 55' X 75' IS OUTSIDE EXISTING FENCE	1991
AWOS	-	-	CURRENT Siting OF THE AWOS DOES NOT MEET DESIGN CRITERIA FOR WIND SENSOR LOCATION	-

CERTIFICATION:  
I HEREBY CERTIFY THAT ALL AIRPORT ELEMENTS SHOWN ON THIS ALP ARE IN ACCORDANCE WITH CRITERIA CONTAINED IN THE CURRENT EDITION OF FAA ADVISORY CIRCULAR 150/5300-13 EXCEPT AS NOTED IN THE TABLE.

*Michael S. Deletsky*  
MICHAEL S. DELETSKY P.E. NO. 5023

*25 Sept 2012*  
DATE

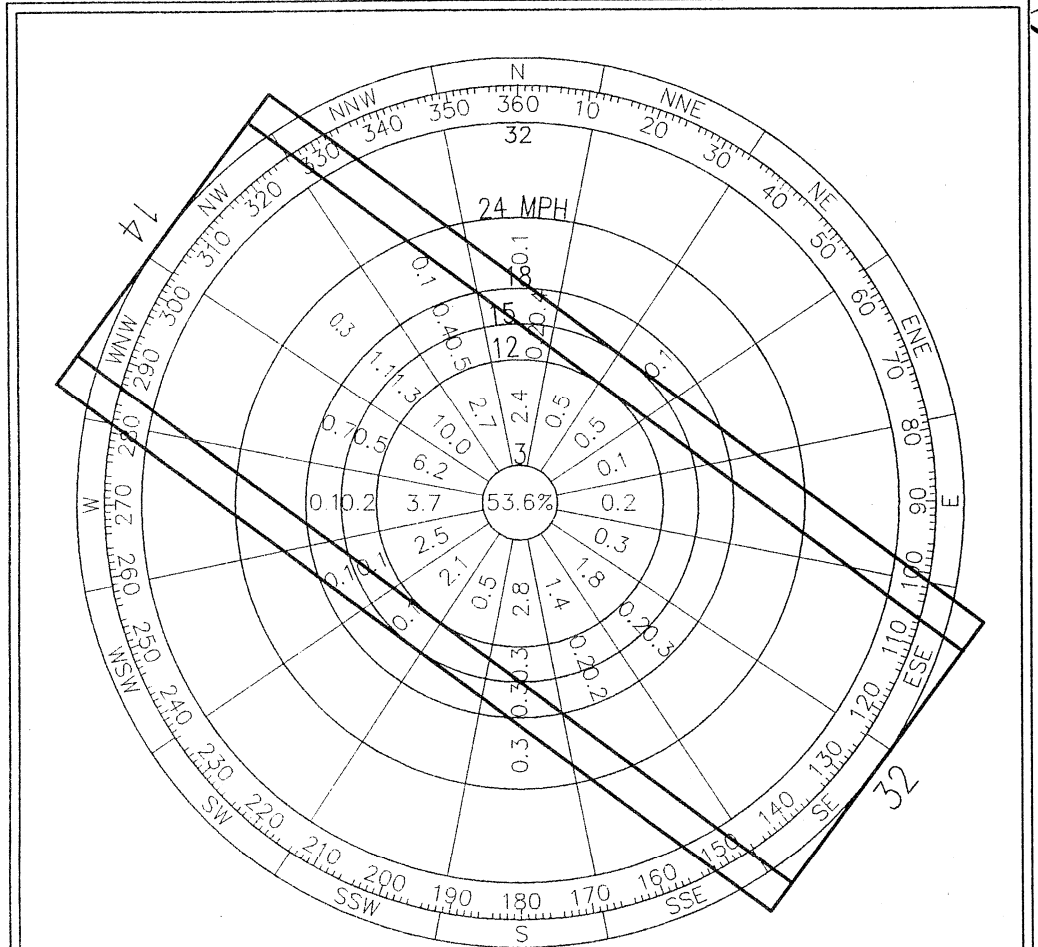
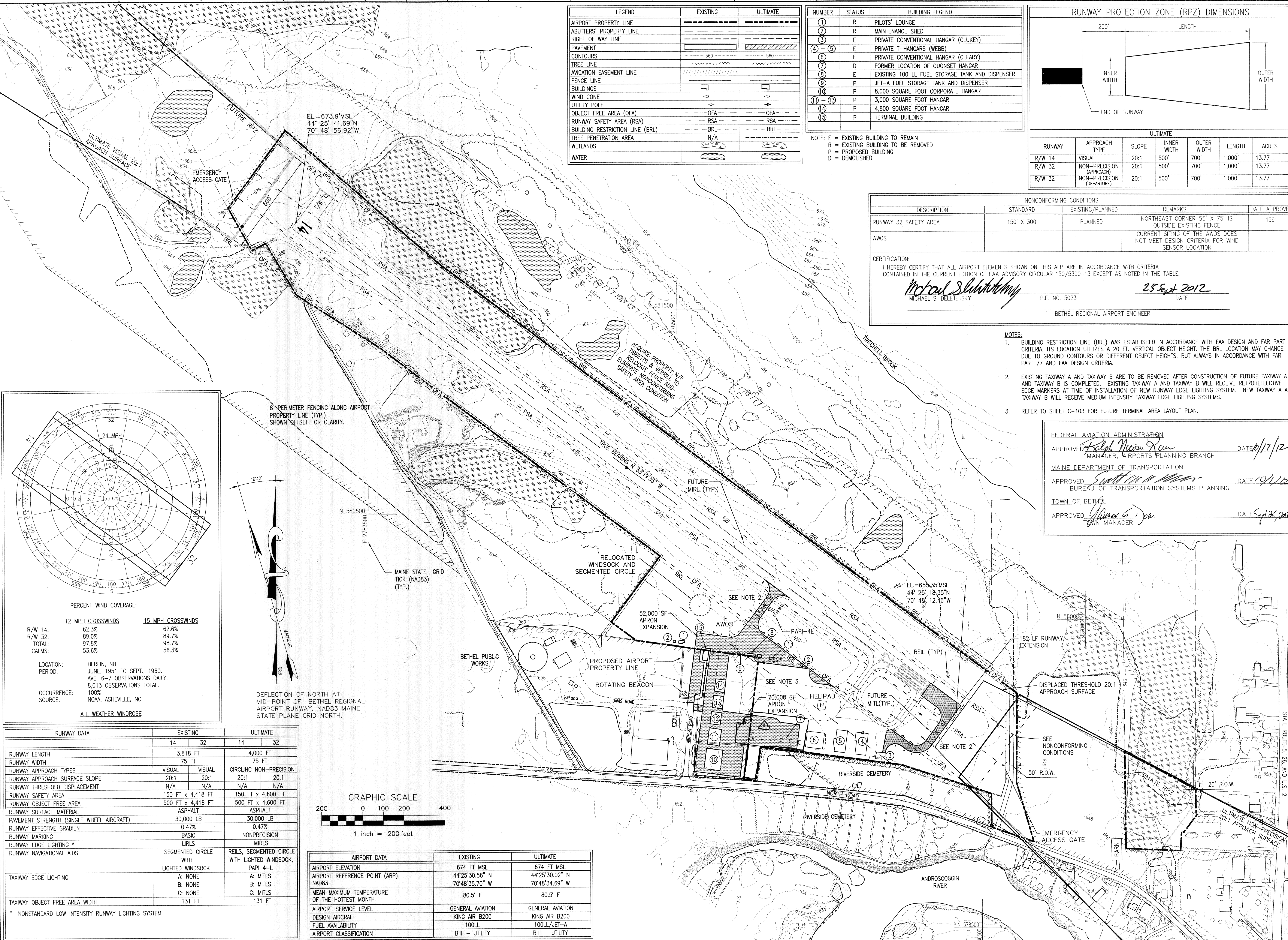
BETHEL REGIONAL AIRPORT ENGINEER

- NOTES:
- BUILDING RESTRICTION LINE (BRL) WAS ESTABLISHED IN ACCORDANCE WITH FAA DESIGN AND FAR PART 77 CRITERIA. ITS LOCATION UTILIZES A 20 FT. VERTICAL OBJECT HEIGHT. THE BRL LOCATION MAY CHANGE DUE TO GROUND CONTOURS OR DIFFERENT OBJECT HEIGHTS, BUT ALWAYS IN ACCORDANCE WITH FAR PART 77 AND FAA DESIGN CRITERIA.
  - EXISTING TAXIWAY A AND TAXIWAY B ARE TO BE REMOVED AFTER CONSTRUCTION OF FUTURE TAXIWAY A AND TAXIWAY B IS COMPLETED. EXISTING TAXIWAY A AND TAXIWAY B WILL RECEIVE RETROREFLECTIVE EDGE MARKERS AT TIME OF INSTALLATION OF NEW RUNWAY EDGE LIGHTING SYSTEM. NEW TAXIWAY A AND TAXIWAY B WILL RECEIVE MEDIUM INTENSITY TAXIWAY EDGE LIGHTING SYSTEMS.
  - REFER TO SHEET C-103 FOR FUTURE TERMINAL AREA LAYOUT PLAN.

FEDERAL AVIATION ADMINISTRATION  
APPROVED *John Moran* DATE *10/17/12*  
MANAGER, AIRPORTS PLANNING BRANCH

MAINE DEPARTMENT OF TRANSPORTATION  
APPROVED *William G. Jones* DATE *10/1/12*  
BUREAU OF TRANSPORTATION SYSTEMS PLANNING

TOWN OF BETHEL  
APPROVED *William G. Jones* DATE *Sept 28, 2012*  
TOWN MANAGER

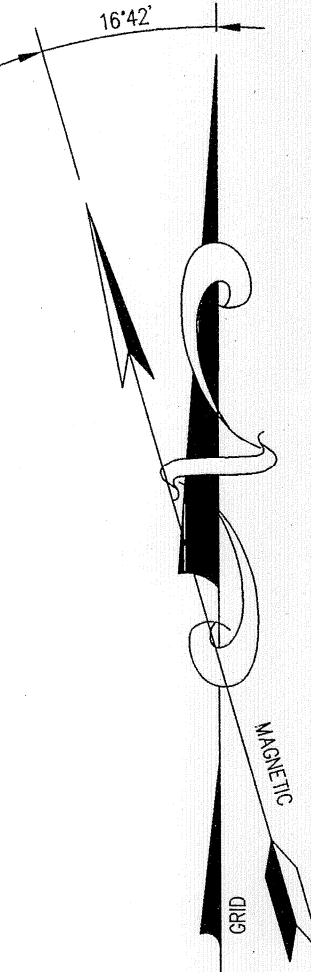


PERCENT WIND COVERAGE:

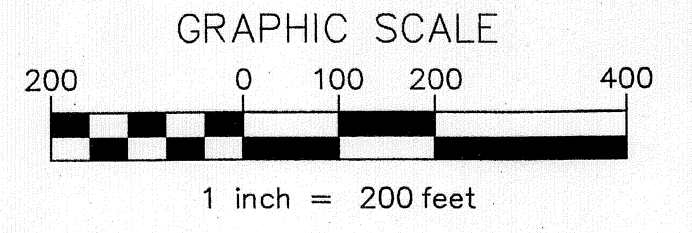
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CALMS:	53.6%	56.3%

LOCATION: BERLIN, NH  
PERIOD: JUNE, 1951 TO SEPT., 1960.  
AVE. 6-7 OBSERVATIONS DAILY.  
8,013 OBSERVATIONS TOTAL.  
OCCURRENCE: 100%  
SOURCE: NOAA, ASHEVILLE, NC

ALL WEATHER WINDROSE



DEFLECTION OF NORTH AT MID-POINT OF BETHEL REGIONAL AIRPORT RUNWAY, NAD83 MAINE STATE PLANE GRID NORTH.



RUNWAY DATA	EXISTING		ULTIMATE	
	14	32	14	32
RUNWAY LENGTH	3,818 FT		4,000 FT	
RUNWAY WIDTH	75 FT		75 FT	
RUNWAY APPROACH TYPES	VISUAL	VISUAL	CIRCLING NON-PRECISION	CIRCLING NON-PRECISION
RUNWAY APPROACH SURFACE SLOPE	20:1	20:1	20:1	20:1
RUNWAY THRESHOLD DISPLACEMENT	N/A	N/A	N/A	N/A
RUNWAY SAFETY AREA	150 FT x 4,418 FT		150 FT x 4,600 FT	
RUNWAY OBJECT FREE AREA	500 FT x 4,418 FT		500 FT x 4,600 FT	
RUNWAY SURFACE MATERIAL	ASPHALT		ASPHALT	
PAVEMENT STRENGTH (SINGLE WHEEL AIRCRAFT)	30,000 LB		30,000 LB	
RUNWAY EFFECTIVE GRADIENT	0.47%		0.47%	
RUNWAY MARKING	BASIC		NONPRECISION	
RUNWAY EDGE LIGHTING *	LIRLS		MIRLS	
RUNWAY NAVIGATIONAL AIDS	SEGMENTED CIRCLE WITH LIGHTED WINDSOCK		REILS, SEGMENTED CIRCLE WITH LIGHTED WINDSOCK, PAPI 4-L	
TAXIWAY EDGE LIGHTING	A: MITLS B: NONE C: NONE		A: MITLS B: MITLS C: MITLS	
TAXIWAY OBJECT FREE AREA WIDTH	131 FT		131 FT	

\* NONSTANDARD LOW INTENSITY RUNWAY LIGHTING SYSTEM

AIRPORT DATA	EXISTING	ULTIMATE
AIRPORT ELEVATION	674 FT MSL	674 FT MSL
AIRPORT REFERENCE POINT (ARP) NAD83	44°25'30.56" N 70°48'35.70" W	44°25'30.02" N 70°48'34.69" W
MEAN MAXIMUM TEMPERATURE OF THE HOTTEST MONTH	80.5° F	80.5° F
AIRPORT SERVICE LEVEL	GENERAL AVIATION	GENERAL AVIATION
DESIGN AIRCRAFT	KING AIR B200	KING AIR B200
FUEL AVAILABILITY	100LL	100LL/JET-A
AIRPORT CLASSIFICATION	B II - UTILITY	B II - UTILITY

**ameco**  
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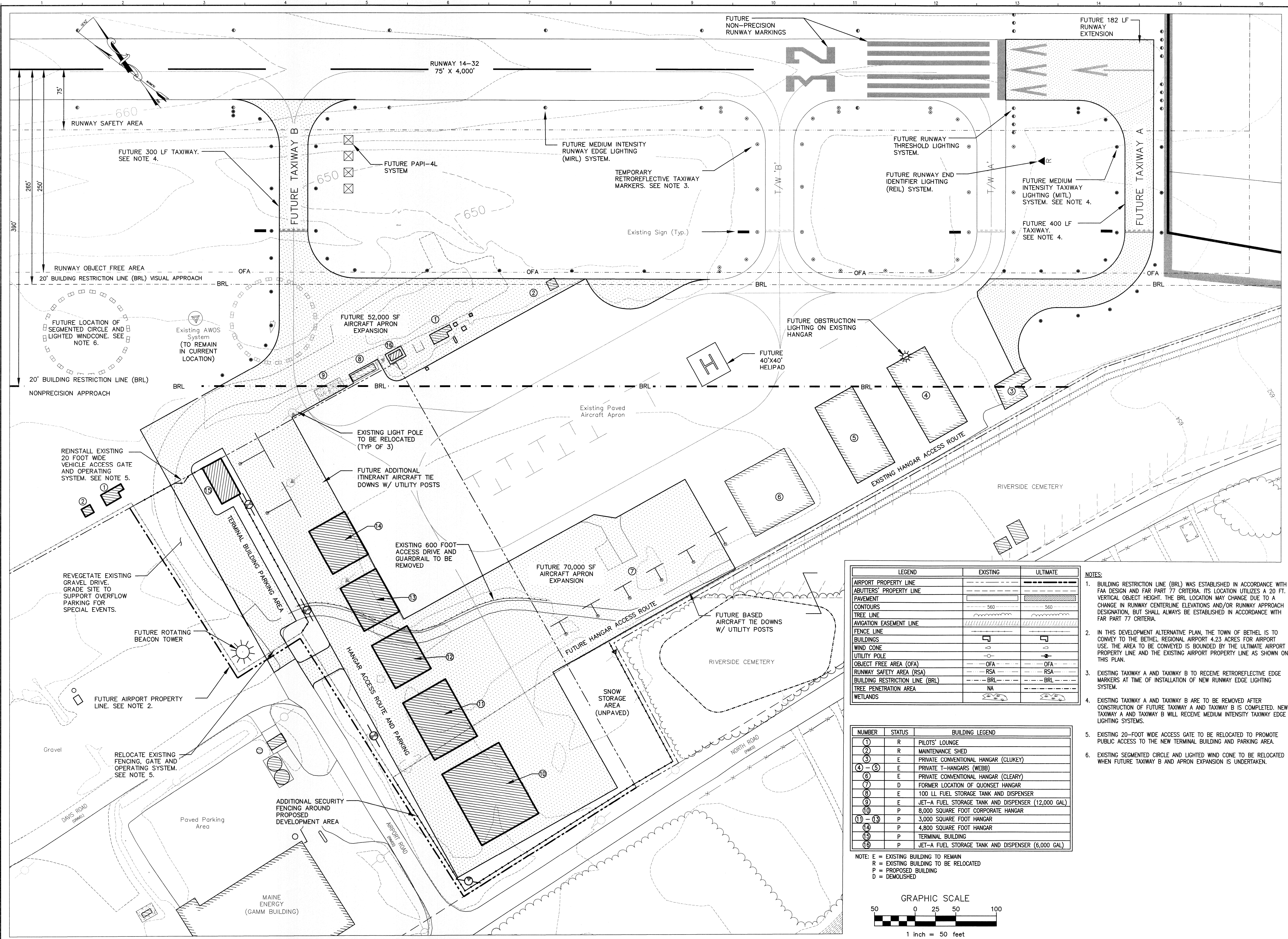
FINAL  
FEBRUARY 2011

BETHEL REGIONAL AIRPORT  
AIP PROJECT NO. 3-23-0008-10-2009  
BETHEL, MAINE

PROJECT NUMBER: 2240201  
PROJECT MANAGER: RDY  
A/E OF RECORD: RDY  
DRAWN BY: DAB  
CAD FILE: 2240201-C-102  
SCALE: AS NOTED

TITLE: ULTIMATE AIRPORT LAYOUT PLAN  
DRAWING NO: C-102  
SHEET: 3 OF 8



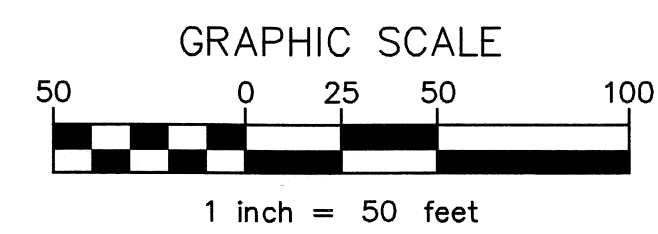


LEGEND	EXISTING	ULTIMATE
AIRPORT PROPERTY LINE	---	---
ABUTTERS' PROPERTY LINE	---	---
PAVEMENT	---	---
CONTOURS	---	---
TREE LINE	---	---
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BUILDING RESTRICTION LINE (BRL)	---	---
TREE PENETRATION AREA	---	---
WETLANDS	---	---

- NOTES:
- BUILDING RESTRICTION LINE (BRL) WAS ESTABLISHED IN ACCORDANCE WITH FAA DESIGN AND FAR PART 77 CRITERIA. ITS LOCATION UTILIZES A 20 FT. VERTICAL OBJECT HEIGHT. THE BRL LOCATION MAY CHANGE DUE TO A CHANGE IN RUNWAY CENTERLINE ELEVATIONS AND/OR RUNWAY APPROACH DESIGNATION, BUT SHALL ALWAYS BE ESTABLISHED IN ACCORDANCE WITH FAR PART 77 CRITERIA.
  - IN THIS DEVELOPMENT ALTERNATIVE PLAN, THE TOWN OF BETHEL IS TO CONVEY TO THE BETHEL REGIONAL AIRPORT 4.23 ACRES FOR AIRPORT USE. THE AREA TO BE CONVEYED IS BOUNDED BY THE ULTIMATE AIRPORT PROPERTY LINE AND THE EXISTING AIRPORT PROPERTY LINE AS SHOWN ON THIS PLAN.
  - EXISTING TAXIWAY A AND TAXIWAY B TO RECEIVE RETROREFLECTIVE EDGE MARKERS AT TIME OF INSTALLATION OF NEW RUNWAY EDGE LIGHTING SYSTEM.
  - EXISTING TAXIWAY A AND TAXIWAY B ARE TO BE REMOVED AFTER CONSTRUCTION OF FUTURE TAXIWAY A AND TAXIWAY B IS COMPLETED. NEW TAXIWAY A AND TAXIWAY B WILL RECEIVE MEDIUM INTENSITY TAXIWAY EDGE LIGHTING SYSTEMS.
  - EXISTING 20-FOOT WIDE ACCESS GATE TO BE RELOCATED TO PROMOTE PUBLIC ACCESS TO THE NEW TERMINAL BUILDING AND PARKING AREA.
  - EXISTING SEGMENTED CIRCLE AND LIGHTED WIND CONE TO BE RELOCATED WHEN FUTURE TAXIWAY B AND APRON EXPANSION IS UNDERTAKEN.

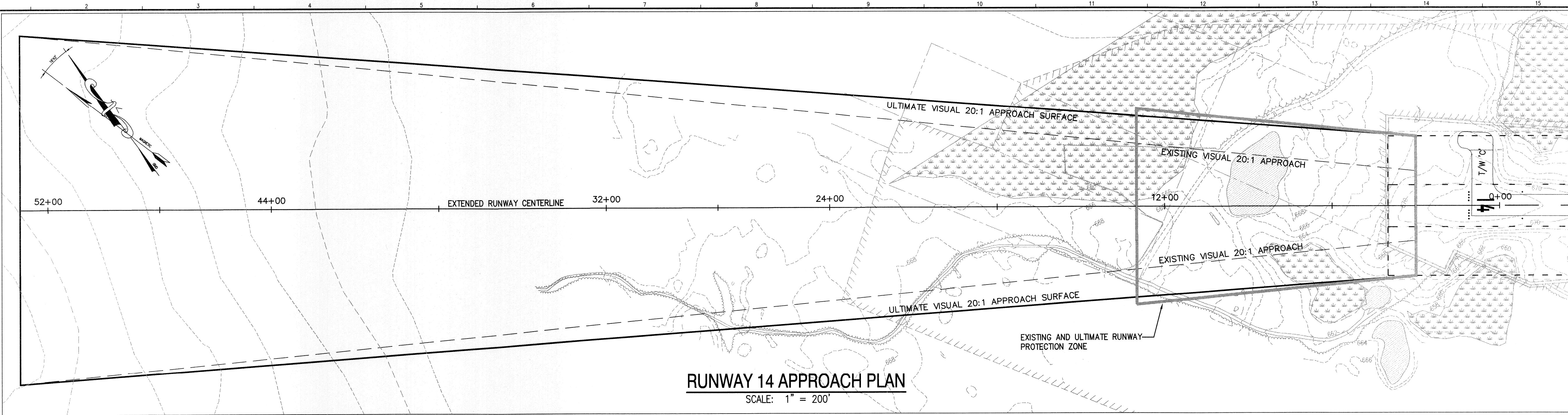
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1	R	PILOTS' LOUNGE
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3	E	PRIVATE CONVENTIONAL HANGAR (CLUKEY)
4-5	E	PRIVATE T-HANGARS (WEBB)
6	E	PRIVATE CONVENTIONAL HANGAR (CLEARY)
7	D	FORMER LOCATION OF QUONSET HANGAR
8	E	100 LL FUEL STORAGE TANK AND DISPENSER
9	E	JET-A FUEL STORAGE TANK AND DISPENSER (12,000 GAL)
10	P	8,000 SQUARE FOOT CORPORATE HANGAR
11-13	P	3,000 SQUARE FOOT HANGAR
14	P	4,800 SQUARE FOOT HANGAR
15	P	TERMINAL BUILDING
16	P	JET-A FUEL STORAGE TANK AND DISPENSER (6,000 GAL)

NOTE: E = EXISTING BUILDING TO REMAIN  
R = EXISTING BUILDING TO BE RELOCATED  
P = PROPOSED BUILDING  
D = DEMOLISHED

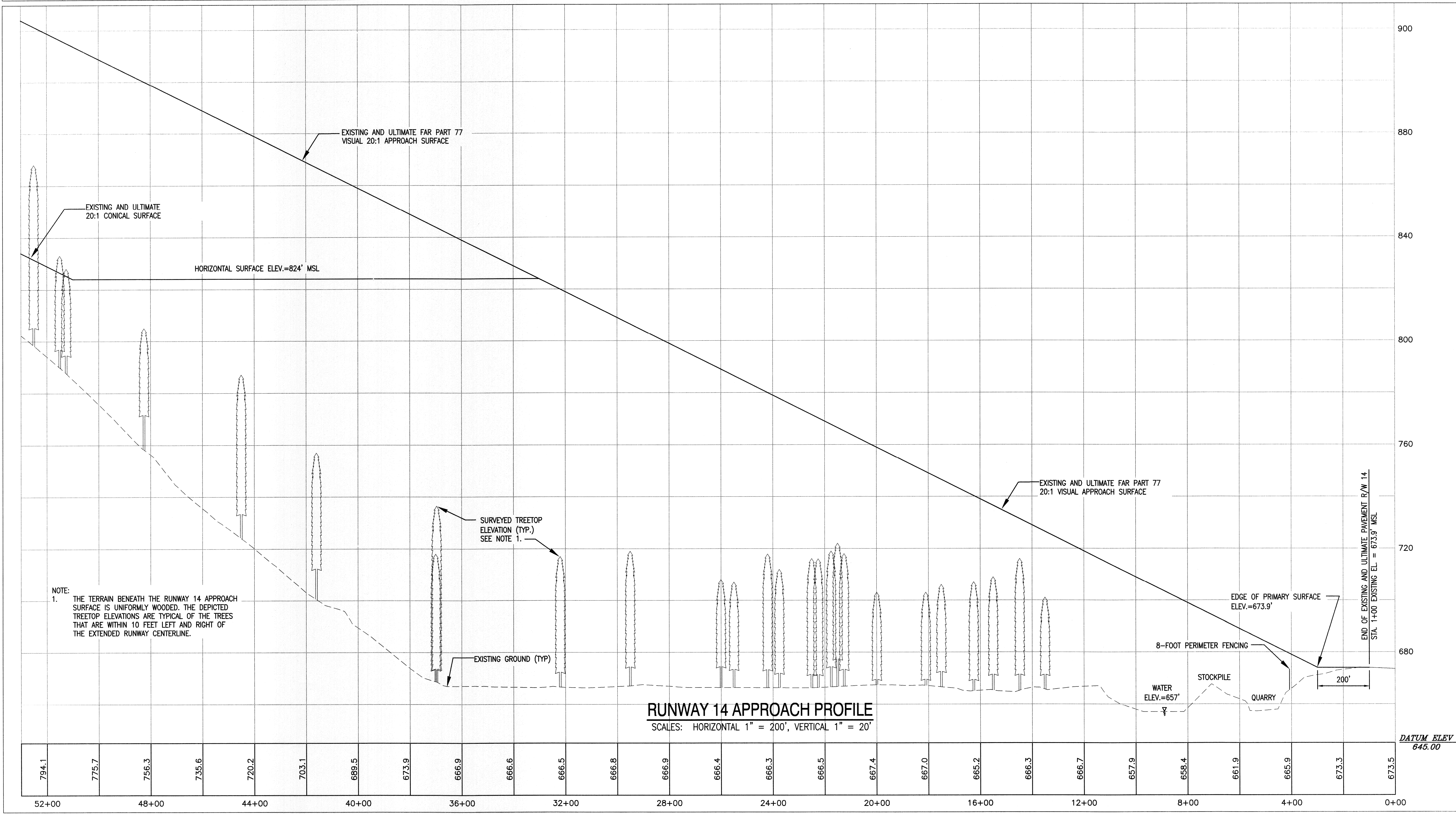


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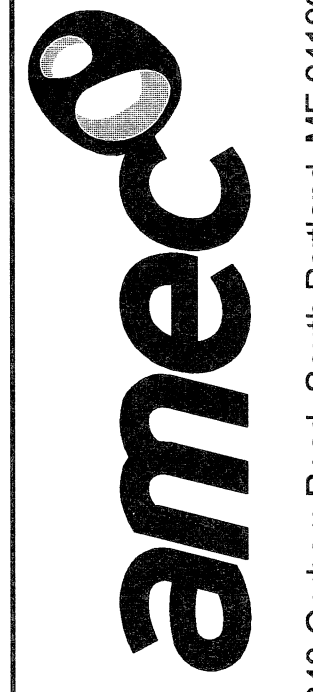
**RUNWAY 14 APPROACH PLAN**  
SCALE: 1" = 200'



**RUNWAY 14 APPROACH PROFILE**  
SCALES: HORIZONTAL 1" = 200', VERTICAL 1" = 20'

NOTE:  
1. THE TERRAIN BENEATH THE RUNWAY 14 APPROACH SURFACE IS UNIFORMLY WOODED. THE DEPICTED TREETOP ELEVATIONS ARE TYPICAL OF THE TREES THAT ARE WITHIN 10 FEET LEFT AND RIGHT OF THE EXTENDED RUNWAY CENTERLINE.

794.1	775.7	756.3	735.6	720.2	703.1	689.5	673.9	666.9	666.6	666.5	666.8	666.9	666.4	666.3	666.5	667.4	667.0	665.2	666.3	666.7	657.9	658.4	661.9	665.9	673.3	673.5
52+00	48+00	44+00	40+00	36+00	32+00	28+00	24+00	20+00	16+00	12+00	8+00	4+00	0+00													



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CURRENT ISSUE STATUS:  
**FINAL**  
**FEBRUARY 2011**  
PROJECT:  
**BETHEL REGIONAL AIRPORT**  
**AIP PROJECT NO. 3-23-0008-10-2009**  
BETHEL, MAINE

REVISION	ISSUE DESCRIPTION	DATE

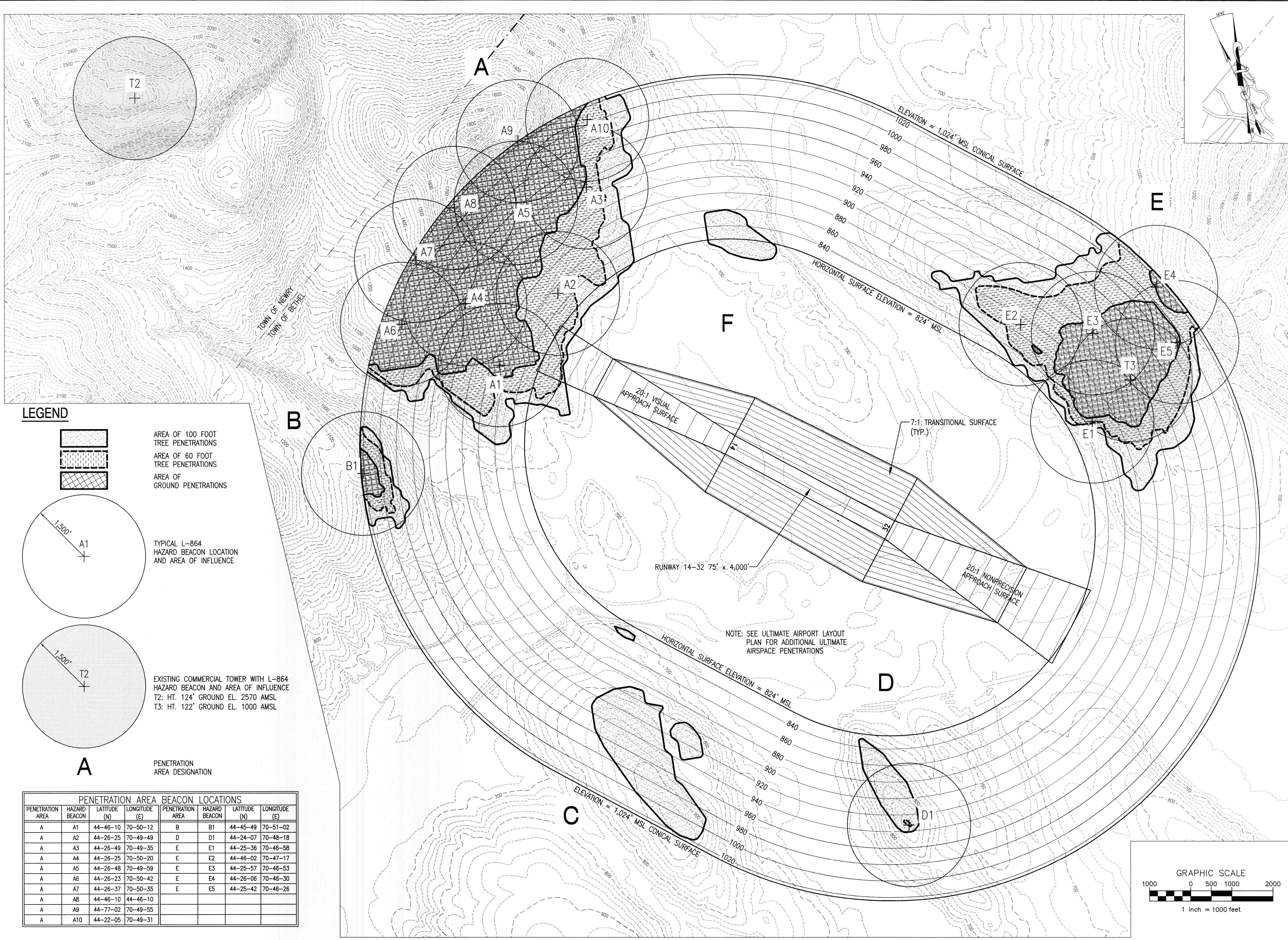
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PROJECT MANAGER: ROY  
A/E OF RECORD: ROY  
DRAWN BY: DAB  
CAD FILE: 2240201-C-104  
SCALE: 1"=200'  
GRAPHIC SCALE:  
0" 1"  
TITLE:  
**RUNWAY 14**  
**APPROACH PLAN**  
**AND PROFILE**  
DRAWING NO:  
**C-104**  
SHEET: 6 OF 8

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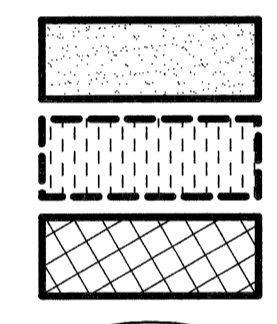




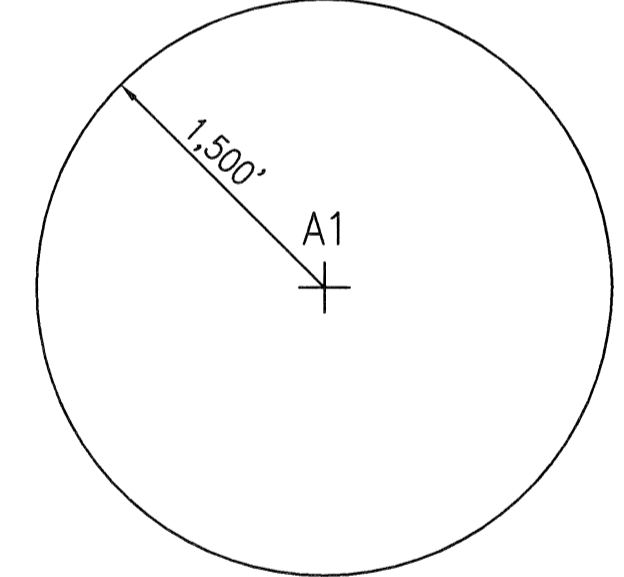




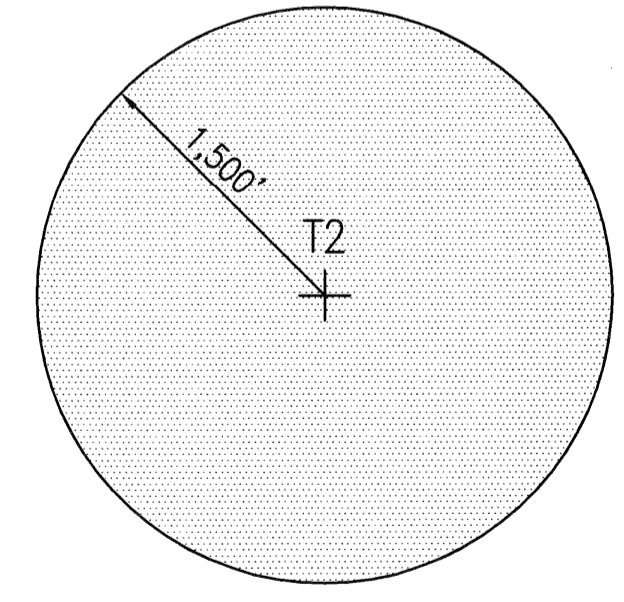
**LEGEND**



AREA OF 100 FOOT TREE PENETRATIONS  
 AREA OF 60 FOOT TREE PENETRATIONS  
 AREA OF GROUND PENETRATIONS



TYPICAL L-864 HAZARD BEACON LOCATION AND AREA OF INFLUENCE



EXISTING COMMERCIAL TOWER WITH L-864 HAZARD BEACON AND AREA OF INFLUENCE  
 T2: HT. 124' GROUND EL. 2570 AMSL  
 T3: HT. 122' GROUND EL. 1000 AMSL

**A** PENETRATION AREA DESIGNATION

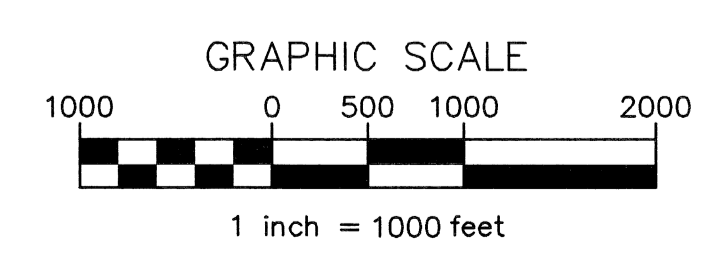
PENETRATION AREA				BEACON LOCATIONS			
PENETRATION AREA	HAZARD BEACON	LATITUDE (N)	LONGITUDE (E)	PENETRATION AREA	HAZARD BEACON	LATITUDE (N)	LONGITUDE (E)
A	A1	44-46-10	70-50-12	B	B1	44-45-49	70-51-02
A	A2	44-26-25	70-49-49	D	D1	44-24-07	70-48-18
A	A3	44-26-49	70-49-35	E	E1	44-25-36	70-46-58
A	A4	44-26-25	70-50-20	E	E2	44-46-02	70-47-17
A	A5	44-26-48	70-49-59	E	E3	44-25-57	70-46-53
A	A6	44-26-23	70-50-42	E	E4	44-26-06	70-46-30
A	A7	44-26-37	70-50-35	E	E5	44-25-42	70-46-26
A	A8	44-46-10	44-46-10				
A	A9	44-77-02	70-49-55				
A	A10	44-22-05	70-49-31				

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PROJECT:  
**BETHEL REGIONAL AIRPORT**  
**AIP PROJECT NO. 3-23-0008-10-2009**  
 BETHEL, MAINE

REV	REVISION	ISSUE DESCRIPTION	DATE

PROJECT NUMBER: 2240201  
 PROJECT MANAGER: RBY  
 A/E OF RECORD: RBY  
 DRAWN BY: DAB  
 CAD FILE: 2240201-C-106  
 SCALE: 1" = 1000'



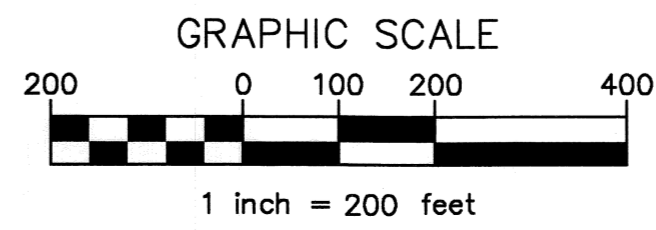
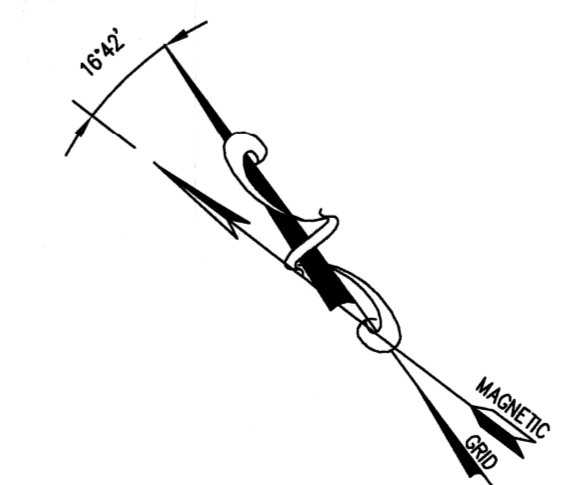
GRAPHIC SCALE:  
 0" 1"  
 1000 0 500 1000 2000  
 1 inch = 1000 feet

TITLE:  
**FAR PART 77**  
**IMAGINARY SURFACES**  
**PLAN**

DRAWING NO:  
**C-106**

SHEET: 7 OF 8



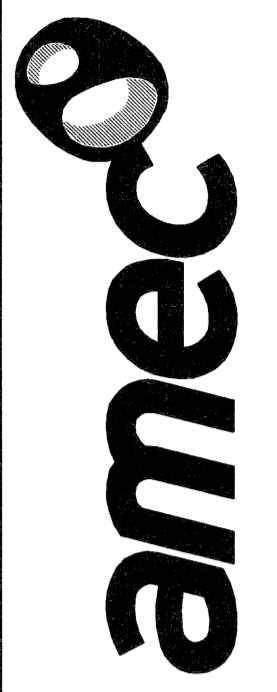


**NOTES:**

- AS OF JUNE, 2010 THE TOWN OF BETHEL HAS NO ADOPTED ZONING ORDINANCES AND VARIES IN USE FROM RESIDENTIAL, OFFICE, LIGHT COMMERCIAL AND GRAVEL EXTRACTION.
- DATA SOURCE: BASE IMAGE BY KAPPA MAPPING, INC. DATED OCTOBER 2009.

**LEGEND:**

- ABUTTING PROPERTY LINE
- AIRPORT PROPERTY LINE
- 55 LDN NOISE CONTOUR



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 PROJECT:  
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 BETHEL, MAINE

REV	REVISION ISSUE DESCRIPTION	DATE

PROJECT NUMBER: 2240201  
 PROJECT MANAGER: RDY  
 A/E OF RECORD: RDY  
 DRAWN BY: DAB  
 CAD FILE: 2240201-C-107  
 SCALE: 1" = 200'

GRAPHIC SCALE:

TITLE:  
**LAND USE PLAN**

DRAWING NO:  
**C-107**  
 SHEET: 8 OF 8

1/24/2011 9:53:33 AM